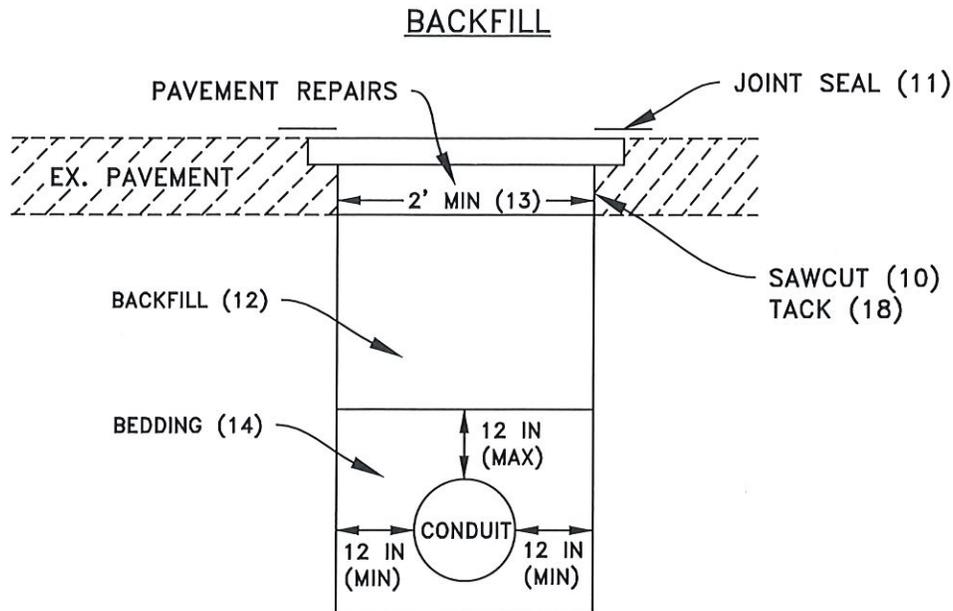


GENERAL NOTES

1. A CONSTRUCTION PERMIT IS REQUIRED FOR ALL EXCAVATIONS WITHIN THE PUBLIC RIGHT-OF-WAY, AS SET FORTH BY LANCASTER CITY CODE, CHAPTER 901.16 AND ISSUED IN ACCORDANCE WITH PROVISIONS IN THE GENERAL RULES AND REGULATIONS OF THE CITY OF LANCASTER ENGINEERING DEPARTMENT.
2. THE CONTRACTOR SHALL COMPLY WITH THE CURRENT CITY OF LANCASTER CONSTRUCTION AND MATERIAL SPECIFICATIONS, INCLUSIVE OF THE OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS, 2019 EDITION.
3. WHEN THE PAVEMENT IS REMOVED AND THE CONTRACTOR IS UNABLE TO COMPLETE THE REQUIRED REPLACEMENT IN TIME FOR IT TO BE OPENED TO TRAFFIC AS INDICATED ON THE PERMIT, THE EXCAVATION SHALL BE FILLED WITH THOROUGHLY COMPACTED BITUMINOUS COLD MIX WITH A MINIMUM OF 2" DURABLE SURFACE (OR APPROVED BITUMINOUS MATERIAL) OR PROPERLY PLATED. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THESE TEMPORARY MEASURES WHILE THEY ARE IN SERVICE.
4. PERMANENT RESTORATION OF ANY SIDEWALK, CURB, STREET PAVEMENT (INCLUDING CRACK SEALING OR HEAT WELDING), ETC., SHALL OCCUR NO LATER THAN 30 DAYS AFTER CONCLUSION OF ANY UTILITY REPAIR OR INSTALLATION ACTIVITY. CONSTRUCTION ACTIVITY COMPLETED NOVEMBER THROUGH APRIL SHALL BE RESOLVED NO LATER THAN MAY 31ST.
5. NO PERMIT SHALL BE GRANTED FOR CUTTING A STREET THAT WAS RESURFACED LESS THAN THREE (3) YEARS PRIOR, EXCEPT FOR THE PURPOSE OF REPAIRING LEAKING PIPES OR WORK DEEMED NECESSARY BY THE DIRECTOR OF PUBLIC SERVICE, CITY ENGINEER OR DESIGNEE. EMERGENCY REPAIRS OR PAVEMENT OPENINGS WITHIN THREE (3) YEARS MAY HAVE ADDITIONAL AND SPECIFIC REQUIREMENTS.
6. CURB RAMPS SHALL BE INSTALLED PER THE CITY OF LANCASTER STANDARD DRAWING P-8 AND MEET ADA RULES AND REGULATIONS.
7. PAVEMENT MARKINGS SHALL BE REPLACED IN LIKE KIND PER; ODOT ITEM 642 TRAFFIC PAINT, ODOT ITEM 644 THERMOPLASTIC, ODOT ITEM 645 PREFORMED OR ODOT ITEM 646 EPOXY (CONCRETE SURFACES) WITHIN 30 DAYS OF THE PAVEMENT REPAIR, OR BY MAY 31ST FOR WINTER REPAIRS. TEMPORARY MARKINGS MAY BE REQUIRED AS DETERMINED BY THE ENGINEER.
8. THE TYPICAL STREET TYPES FOUND IN THE CITY CAN BE CLASSIFIED AS FOLLOWS: FLEXIBLE PAVEMENT MATERIALS ARE AGGREGATES, BITUMINOUS MIXTURES AND ASPHALT; RIGID PAVEMENT CONSISTS OF CONCRETE, CEMENT MIXTURES AND BRICK; COMPOSITE PAVEMENTS TYPICALLY CONSIST OF A FLEXIBLE SURFACE WITH A RIGID BASE; BRICK PAVEMENTS ARE TYPICALLY A BRICK SURFACE ON A RIGID BASE PAVEMENT REPAIRS SHALL CONFORM TO THE TYPE AND THICKNESS OF THE EXISTING PAVEMENT.
9. FOR ALLEY AND DRIVEWAY REPAIRS, THE PAVEMENT REPLACEMENT SHALL CONFORM TO THE TYPE AND THICKNESS OF THE EXISTING PAVEMENT. THE MINIMUM PAVEMENT THICKNESS, REGARDLESS OF TYPE, SHALL BE 6 INCHES. IF MORE THAN 1/3 OF THE WIDTH OF AN ALLEY IS REMOVED, THE ENTIRE WIDTH SHALL BE RESURFACED.
10. SAWCUT THE EXISTING PAVEMENT AT EXCAVATION LIMITS AND AS NECESSARY TO PREVENT DISTURBING OR UNDERMINING THE REMAINING PAVEMENT. PROCEDURES USED FOR THE PAVEMENT REMOVAL AND REPLACEMENT SHALL NOT CAUSE SPALLING OR CRACKING OF THE ADJACENT PAVEMENT.
11. FOR PAVEMENT REPAIR JOINT SEALING, THE FOLLOWING METHODS ARE PERMITTED:
 - A. THE PREFERRED METHOD IS TO SEAL THE PERIMETER SURFACE OF THE REPAIRED AREA BY APPLYING A 4 INCH STRIP OF ODOT ITEM 423 CRACK SEALING.
 - B. A SECOND OPTION IS TO HEAT WELD A 12 INCH WIDE STRIP, CENTERED ON THE CUT, TO A DEPTH OF 2 INCHES, PER ODOT ITEM 423.
 - C. KOLD-FLO® POURABLE CRACK FILLER MAY BE USED AROUND SMALL REPAIR AREAS WITH A PERIMETER LESS THAN 50 LINEAR FEET. THE APPLICATION SHALL FOLLOW THE MANUFACTURES RECOMMENDATIONS. OTHER MODIFIED ASPHALT EMULSION SEALING PRODUCTS MAY BE USED WITH THE ENGINEERS APPROVAL.
 - D. QUIKJOINT TAPE MAY BE USED AND INSTALLED PER MANUFACTURES SPECIFICATIONS AND RECOMMENDATIONS.

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| APPROVED <u>1/27/23</u>  CITY ENGINEER | PAVEMENT REPAIR GENERAL NOTES | CITY OF LANCASTER, OHIO DEPARTMENT OF ENGINEERING STANDARD CONSTRUCTION DRAWING DWG. BY: ...ADH..... CHK'D BY: ...CMS..... |
| REVISED: 27 JAN 23 | | FILE NUMBER P-1 1 of 10 |



12. FOR BACKFILLING UNDER THE INFLUENCE LINE (SEE P-28) OF ALL PAVED SURFACES, THE FOLLOWING OPTIONS ARE PERMITTED:

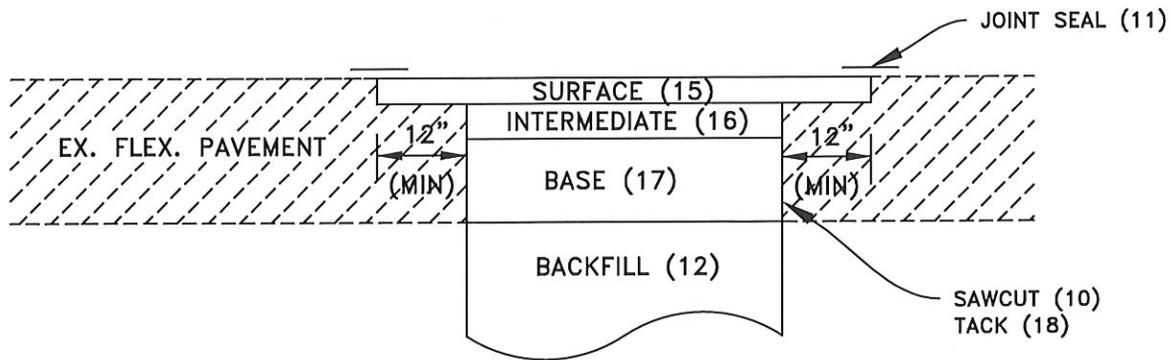
- A. THE PREFERRED BACKFILL IS LANCASTER/ODOT ITEM 613 LOW STRENGTH MORTAR (LSM). NO PAVEMENT SHALL BE PLACED ON THE LSM UNTIL BLEED WATER IS ABSENT FROM THE SURFACE. LSM SHALL BE USED IN SMALL TRENCHES WHERE TYPICAL COMPACTION EQUIPMENT CANNOT BE USED.
- B. COMPACTED GRANULAR BACKFILL PER ODOT ITEM 304, AGGREGATE BASE, MAY BE USED WHERE THE TRENCH WIDTH IS SUFFICIENT TO ACHIEVE PROPER COMPACTION. BEFORE SPREADING SAMPLE THE MATERIAL AND DETERMINE THE OPTIMUM MOISTURE CONTENT PER ODOT ITEM 304.0.3. SPREAD THE MATERIAL ACCORDING TO ODOT ITEM 304.04, IN LIFTS RANGING FROM 4 TO 8 INCHES, DEPENDANT ON THE COMPACTION EQUIPMENT. VIBRATORY EQUIPMENT MAY BE USED IN SMALL AREAS. COMPACTION TESTING MAY BE REQUIRED TO ASSURE THAT 98% OF THE MAXIMUM DRY DENSITY IS ACHIEVED. THIS METHOD REQUIRES FULL TIME CITY INSPECTION.

BACKFILL SHALL NOT EXTEND INTO THE PAVEMENT AND SHALL NOT BE PLACED HIGHER THAN THE SUBGRADE ELEVATION. BACKFILL IS NOT PERMITTED AS A TEMPORARY DRIVING SURFACE.

- 13. THE PAVEMENT PORTION OF THE TRENCH SHALL BE A MINIMUM OF 2 FT IN WIDTH. THIS IS TO ALLOW FOR THE PROPER COMPACTION OF THE ASPHALT PAVEMENT. IF THE TRENCH FOR PLACING CONDUIT IS NARROWER THAN 2 FT THEN THE PAVEMENT PORTION SHALL BE CUT BACK TO PROVIDE THE 2 FT MINIMUM FOR PAVING OPERATIONS.
- 14. BEDDING MATERIAL MAY BE PLACED ACCORDING TO THE UTILITY OWNER AND CONDUIT MANUFACTURERS SPECIFICATIONS AND STANDARDS, HOWEVER IS SUBJECT TO APPROVAL OR DENIAL OF THE ENGINEER, TO PROTECT THE INTEGRITY OF THE STREET FOUNDATION. THE FOLLOWING RESTRICTIONS APPLY:
 - A. BEDDING MAY BE PLACED TO A MAXIMUM OF 12 INCHES ABOVE THE CONDUIT.
 - B. THE TRENCH WIDTH SHALL BE SUFFICIENT TO PROVIDE AT LEAST 12 INCHES ON EITHER SIDE OF THE CONDUIT TO ALLOW FOR THE PROPER COMPACTION OF THE BEDDING MATERIAL.
 - C. THE BEDDING MATERIALS SHALL BE COMPACTIBLE AND INCLUDE SAND, GRANULAR MATERIAL, LOW STRENGTH MORTAR AND CONCRETE. THE BEDDING MATERIAL IS SUBJECT TO APPROVAL BY THE CITY ENGINEER.

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| APPROVED <u>1/27/23</u>  CITY ENGINEER | PAVEMENT REPAIR BACKFILL | CITY OF LANCASTER, OHIO DEPARTMENT OF ENGINEERING | | | | | |
| REVISED: 27 JAN 23 | | <table border="1" style="width: 100%;"> <tr> <td colspan="2" style="text-align: center;">STANDARD CONSTRUCTION DRAWING</td> </tr> <tr> <td>DWG. BY: ...ADH...</td> <td>FILE NUMBER</td> </tr> <tr> <td>CHK'D BY: ...CMS...</td> <td style="text-align: center;">P-1 2 of 10</td> </tr> </table> | STANDARD CONSTRUCTION DRAWING | | DWG. BY: ...ADH... | FILE NUMBER | CHK'D BY: ...CMS... |
| STANDARD CONSTRUCTION DRAWING | | | | | | | |
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FLEXIBLE



THE SURFACE COURSE ON ALL FLEXIBLE AND COMPOSITE PAVEMENTS, SHALL BE 1.5 INCHES OF ODOT ITEM 441 ASPHALT CONCRETE, SURFACE COURSE, TYPE 1. PRIOR TO PLACEMENT, ITEM 407 TACK COAT SHALL BE APPLIED ON THE INTERMEDIATE COURSE AT A RATE OF 0.1 GAL/SY.

THE SURFACE COURSE SHALL EXTEND A MINIMUM OF 12 INCHES BEYOND THE TRENCH WALL, AND SHALL BE OF AN ACCEPTABLE SHAPE AS PRESENTED ON SHEETS 7 & 8 OF THIS DRAWING, AND AS DESCRIBED BELOW:

- A. WHEN AN EXCAVATION EXCEEDS 100 FEET IN LENGTH, THE SURFACE REPAIR SHALL INCLUDE THE FULL LANE WIDTH OF ALL LANES THAT ARE ENCRONCHED UPON.
- B. WHEN AN EXCAVATION CROSSES A LANE AT A FREQUENCY OF 2 OR MORE LATERAL EXCAVATIONS WITHIN 25 FEET OF EACH OTHER, THE SURFACE REPAIR SHALL INCLUDE THE FULL LANE WIDTH OF ALL LANES THAT ARE AFFECTED AND EXTEND 2 FEET BEYOND THE LATERAL EXCAVATIONS.
- C. THE SURFACE REPAIR OF ALL IRREGULAR-SHAPED EXCAVATIONS SHALL ALWAYS BE A RECTANGLE WITH SIDES THAT ARE PARALLEL/PERPENDICULAR TO THE DIRECTION OF TRAVEL OF THE STREET.

TRENCHES THAT REQUIRE FULL LANE RESURFACING SHALL INCLUDE FULL LANE RESURFACING ON ALL CONNECTING TRENCHES AND ASSOCIATED VALVE OR CASTING WORK AREAS ALONG ADJACENT STREETS (UTILITY SERVICE REPAIRS SHALL BE AS PER SHEETS 7 AND 8) REGARDLESS OF THE LENGTH OF THE CONNECTING TRENCH.

16. THE INTERMEDIATE COURSE ON ALL FLEXIBLE AND COMPOSITE PAVEMENTS, SHALL BE 1.75 INCHES OF ODOT ITEM 441 ASPHALT CONCRETE, INTERMEDIATE COURSE, TYPE 2. THE MAXIMUM LIFT THICKNESS IS 3 INCHES. THE MINIMUM COMPACTED DEPTH IS 1.75 INCHES. PRIOR TO PLACEMENT, ITEM 407 TACK COAT SHALL BE APPLIED ON THE BASE COURSE AT A RATE OF 0.15 GAL/SY.

FOR COMPOSITE PAVEMENTS, IF THE MINIMUM DEPTH CANNOT BE ACHIEVED BETWEEN THE RIGID BASE AND THE AC SURFACE, THEN THE SURFACE COURSE SHALL EXTEND TO THE RIGID BASE.

WHERE RESURFACING APPLIES, THE INTERMEDIATE COURSE MAY BE PLACED TO THE FINISHED SURFACE TEMPORARILY UNTIL THE PLANING AND RESURFACING CAN BE PERFORMED. THE INTERMEDIATE COURSE IS NOT PERMITTED AS THE FINAL SURFACE COURSE.

17. THE BASE COURSE FOR FLEXIBLE PAVEMENTS SHALL BE 2" OR 3" LIFTS OF ODOT ITEM 301, ASPHALT CONCRETE BASE TO MAKE A MAXIMUM TOTAL LIFT OF 6". EACH LIFT MAY BE COMPACTED WITH A 3-5 TON ROLLER OR VIBRATORY PLATE COMPACTORS, OR EQUIVALENTS APPROVED BY THE CITY ENGINEER. THE TEMPERATURE OF THE ASPHALT WHEN DELIVERED TO THE PAVER SHALL BE A MINIMUM OF 250° F.

18. TACK COAT SHALL BE APPLIED AT A RATE OF 0.25 GAL/SY TO THE SIDES OF THE SAWN PAVEMENT AND BASE MATERIAL BEFORE PLACING THE ASPHALT CONCRETE. THE TACK COAT SHALL MEET THE REQUIREMENTS OF ITEM 407.

APPROVED 1/27/23

[Signature]
CITY ENGINEER

REVISED: 27 JAN 23

PAVEMENT REPAIR
FLEXIBLE

CITY OF LANCASTER, OHIO
DEPARTMENT OF ENGINEERING

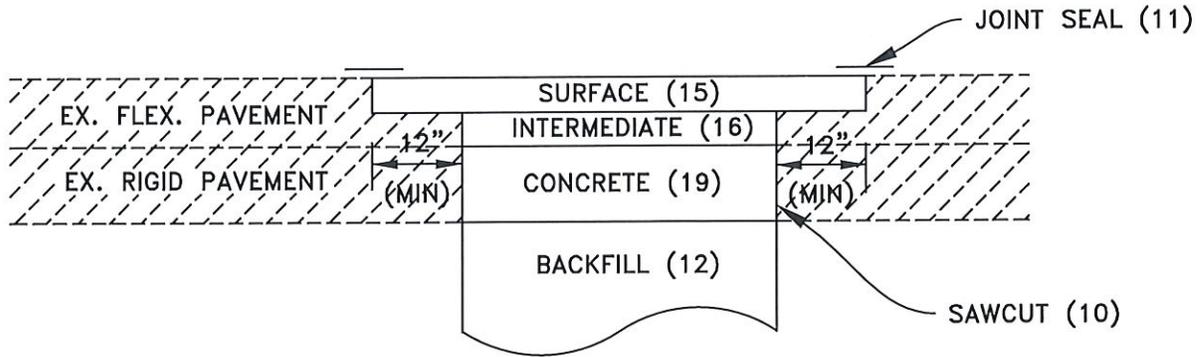
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COMPOSITE



19. THE RIGID BASE FOR COMPOSITE AND BRICK PAVEMENTS SHALL BE ODOT ITEM 305, CONCRETE BASE. THE THICKNESS SHALL MATCH THE EXISTING (6 INCHES MINIMUM) AND IT SHALL BE PLACED TO THE LEVEL OF THE EXISTING ADJACENT CONCRETE BASE.

THE CONCRETE BASE SHALL NOT BE REINFORCED WITH STEEL.

FOR COMPOSITE PAVEMENTS (8), THE CONCRETE BASE (19) MAY BE TEMPORARILY PLACED TO THE SURFACE, TO BE MILLED DOWN TO THE TOP OF THE ADJACENT RIGID BASE. THE INTERMEDIATE AND SURFACE COURSES SHALL BE PLACED IN A APPROPRIATE TIME.

APPROVED 1/27/23

 CITY ENGINEER

PAVEMENT REPAIR
 COMPOSITE

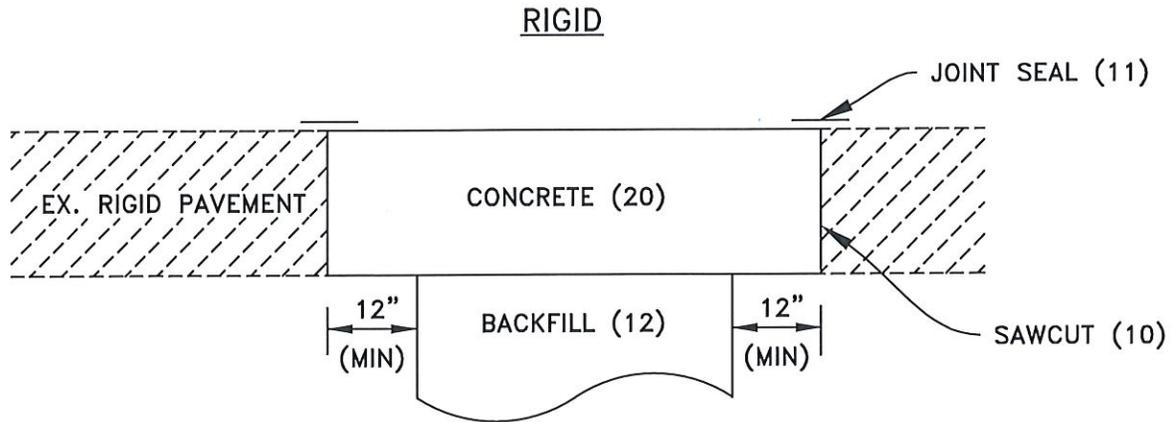
CITY OF LANCASTER, OHIO
 DEPARTMENT OF ENGINEERING

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20. FOR RIGID PAVEMENT REPAIRS, THE REPLACED PAVEMENT SHALL MATCH THE EXISTING THICKNESS AND SHALL BE REINFORCED IF THE EXISTING PAVEMENT IS REINFORCED. THE FOLLOWING OPTIONS ARE PERMITTED:

- A. ODOT ITEM 451, REINFORCED CONCRETE PAVEMENT, CLASS QC1.
- B. ODOT ITEM 452, NON-REINFORCED CONCRETE PAVEMENT, CLASS QC1.

THE LIMITS OF THE CONCRETE REPAIR SHALL EXTEND 12 INCHES (MINIMUM) BEYOND THE TRENCH.

IF THE UTILITY TRENCH CUT IS WITHIN 6 FT OF A TRANSVERSE OR LONGITUDINAL JOINT, THE LIMITS OF THE REPAIR SHALL EXTEND TO THE JOINT. THIS MAY REQUIRE THE ENTIRE PANEL TO BE REPLACED.

IF MAINTENANCE OF TRAFFIC REQUIREMENTS LIMIT THE CURING TIME, FAST SETTING (CLASS QCFS) OR MEDIUM SETTING (CLASS QCMS) MAY BE USED IF APPROVED BY THE ENGINEER.

APPROVED 1/27/23

 CITY ENGINEER

PAVEMENT REPAIR
 RIGID

CITY OF LANCASTER, OHIO
 DEPARTMENT OF ENGINEERING

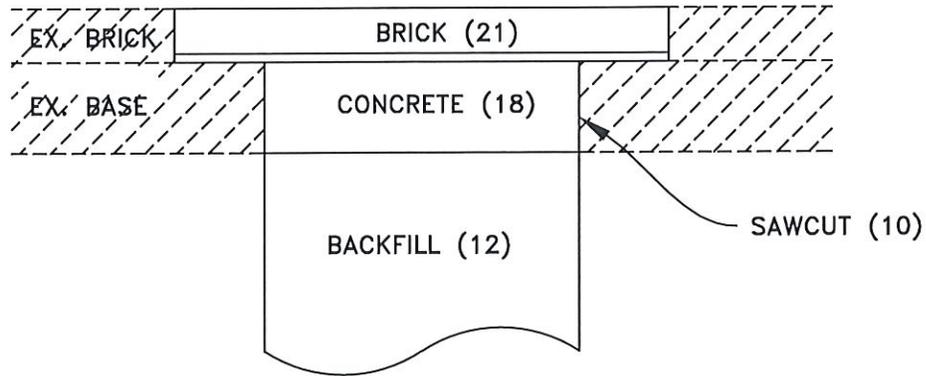
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BRICK



21. WHEN EXCAVATING AND REPAIRING BRICK STREETS, THE MATERIAL USED FOR REPLACEMENT SHALL MATCH THE EXISTING.

BRICKS OR PAVERS REMOVED FROM A REPAIR AREA SHALL BE STORED IN A SAFE PLACE BY THE CONTRACTOR FOR REUSE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING ANY BRICKS OR PAVERS THAT ARE STOLEN OR DAMAGED, AT NO ADDITIONAL COST TO THE CITY.

IF BRICKS OR PAVERS ARE SUPPLIED BY THE CONTRACTOR, THEY MUST CLOSELY MATCH THE EXISTING BRICKS OR PAVERS AND FIRST BE APPROVED BY THE CITY.

SAW CUTTING: ALL PARTIAL BRICKS SHALL BE SAWCUT. FURTHER, NO BRICK WILL BE PERMITTED TO BE CUT, FOR REPLACEMENT, TO A LENGTH LESS THAN 1/2 ITS ORIGINAL LENGTH. THIS MAY REQUIRE SAW CUTTING OF ADJACENT UNDISTURBED BRICK(S).

DURING REMOVAL OF THE EXISTING BASE MATERIAL, IT SHALL BE CUT BACK TO AS NEARLY VERTICAL AS POSSIBLE. IF SHEARING OF THE ADJACENT BASE RESULTS, THE CONTRACTOR SHALL REMOVE ADDITIONAL BASE MATERIAL UNTIL A VERTICAL FACE IS ACHIEVED.

DURING INSTALLATION, THE BRICK IS TO BE RESET IN REASONABLY CLOSE CONFORMITY TO THE PATTERN OF THE EXISTING BRICK PAVEMENT ON A SETTING BED OVER ITEM 305 CONCRETE BASE. THE SETTING BED FOR HISTORICAL BRICK STREETS SHALL CONSIST OF 1 INCH OF SAND; WHEREAS, 3/4-INCH BITUMINOUS SETTING BED FOR NEWER STYLE ROADWAY PAVERS. THE CONCRETE BASE THICKNESS SHALL MATCH THE EXISTING BASE OR A MINIMUM OF 7 INCHES.

HISTORICAL BRICKS WITHOUT SPACING LUGS: THE MAXIMUM WIDTH OF A BRICK JOINT SHALL BE 1/2 INCH. THIS RESTRICTION SHALL ALSO APPLY TO THE JOINT FORMED ADJACENT TO THE PERIMETER OF A REPAIR AREA, WHERE THE ROWS MAY NOT BE PARALLEL TO ONE ANOTHER. ALL JOINTS SHALL BE FILLED WITH POLYMERIC SAND FOLLOWING MANUFACTURER'S INSTRUCTIONS. THIS MAY REQUIRE MORE THAN ONE APPLICATION. FURTHER, MECHANICAL VIBRATION WILL BE REQUIRED FOR CONSOLIDATION OF DRY MORTAR MIX.

APPROVED 1/27/23


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REVISED: 27 JAN 23

PAVEMENT REPAIR
BRICK

CITY OF LANCASTER, OHIO
DEPARTMENT OF ENGINEERING

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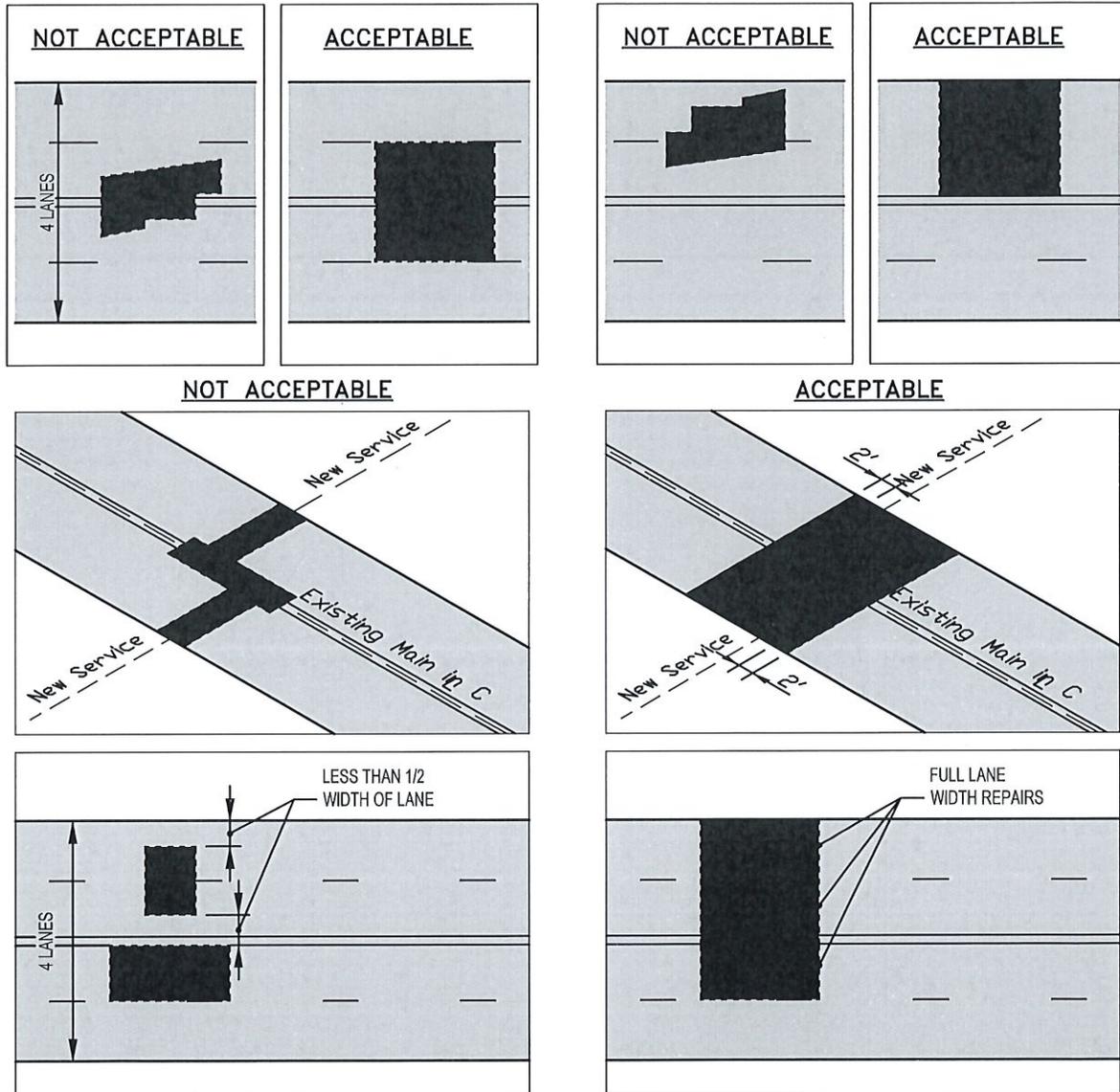
22. AREAS TO BE RESURFACED SHALL BE PLANED ACCORDING TO ODOT ITEM 254 PAVEMENT PLANING, THOROUGHLY CLEANED AND DRIED, THEN TACKED PER ODOT ITEM 407 PRIOR TO PLACING AND COMPACTING THE ASPHALT CONCRETE WITH A ROLLER AND PAVER.

THE RESURFACING SHALL NOT INTRODUCE ANY LONGITUDINAL PAVEMENT JOINTS. WHEN RESURFACING OUTSIDE LANES, RESURFACING SHALL EXTEND TO THE FACE OF CURB OR EDGE OF PAVEMENT.

IF PAVEMENT PLANING DOES NOT PROVIDE A UNIFORM PLANED SURFACE DUE TO THE EXISTING PAVEMENT CONDITION, THE DEPTH OF THE PAVEMENT REMOVAL AND RESURFACING SHALL BE ADJUSTED ACCORDINGLY.

WHERE THE PROPOSED RESURFACING IS IN CLOSE PROXIMITY TO AN EXISTING LONGITUDINAL JOINT, THE RESURFACING SHALL BE EXTENDED TO MEET OR OVERLAP THAT JOINT.

WHEN RESURFACING ADJOINS AN AREA WITH EXISTING OVERLAID GUTTER, THE RESURFACING SHALL EXTEND THE FULL LANE WIDTH TO THE EXISTING PAVEMENT EDGE AT THE FACE OF CURB.



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 CITY ENGINEER

PAVEMENT REPAIR
 RESURFACE

CITY OF LANCASTER, OHIO
 DEPARTMENT OF ENGINEERING

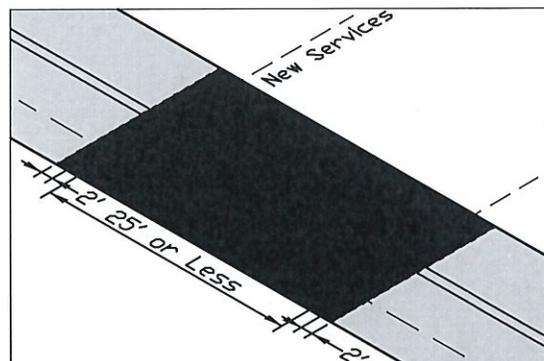
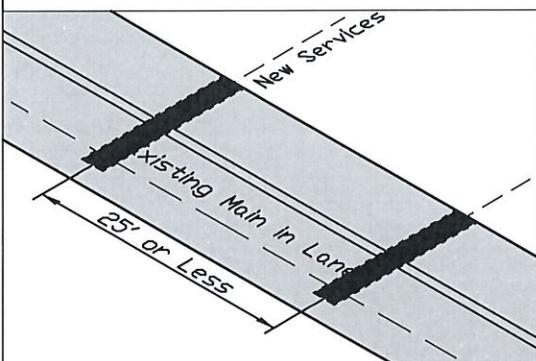
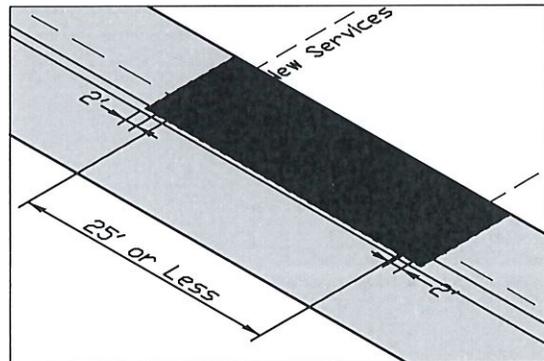
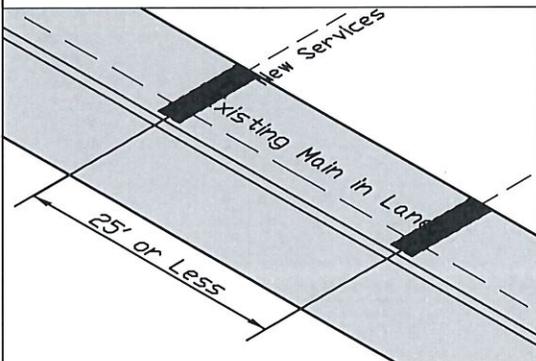
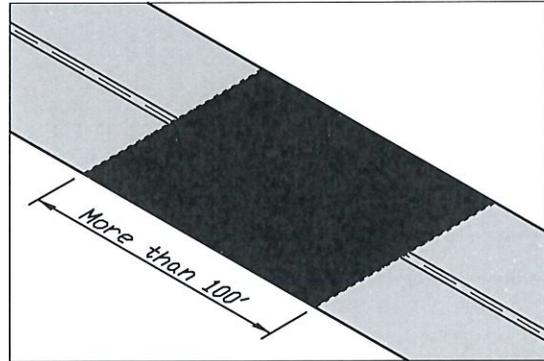
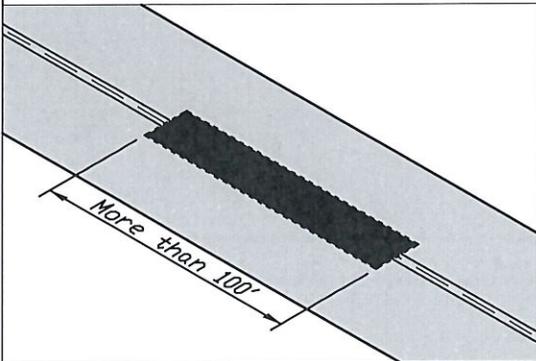
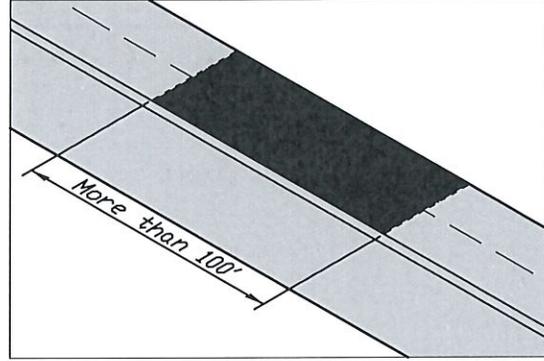
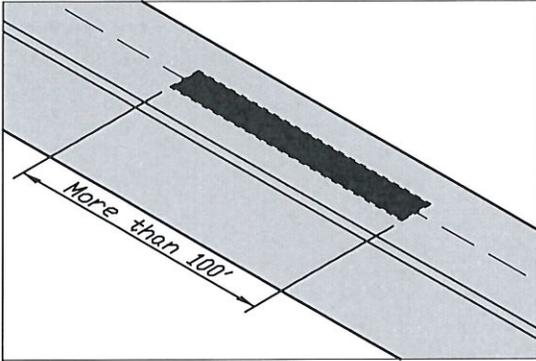
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NOT ACCEPTABLE

ACCEPTABLE



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 CITY ENGINEER

REVISED: 27 JAN 23

PAVEMENT REPAIR
 RESURFACE

CITY OF LANCASTER, OHIO
 DEPARTMENT OF ENGINEERING

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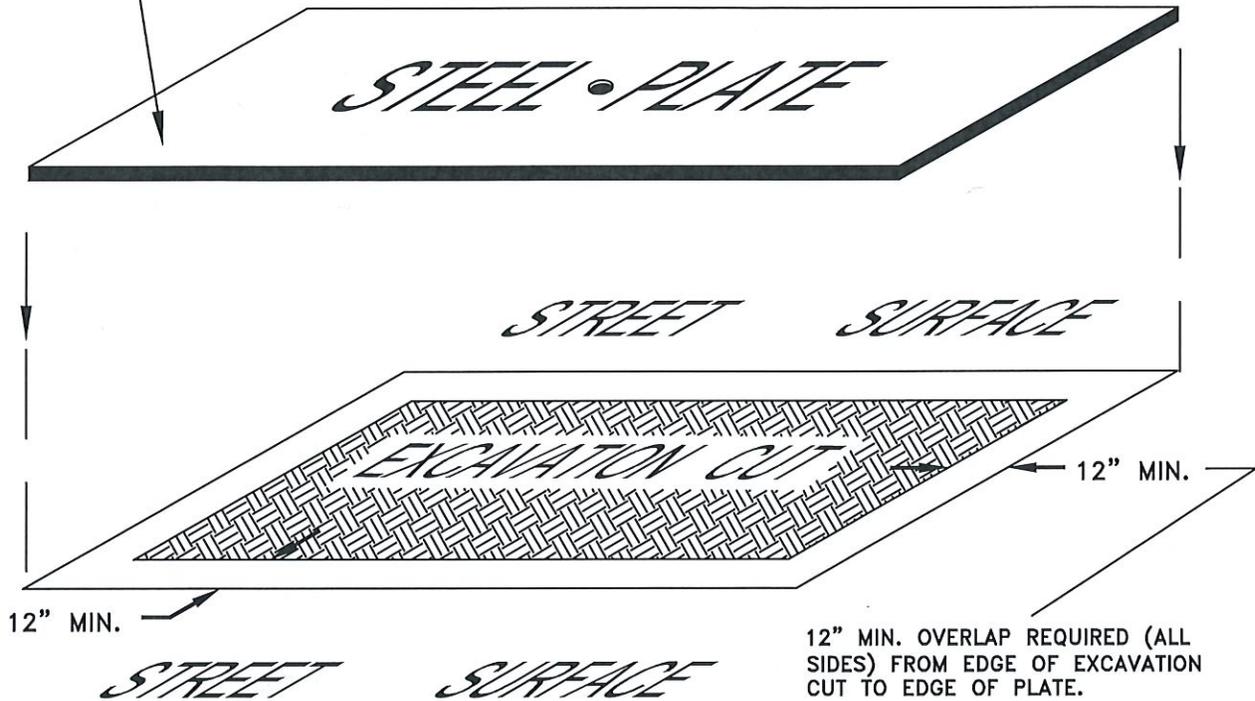
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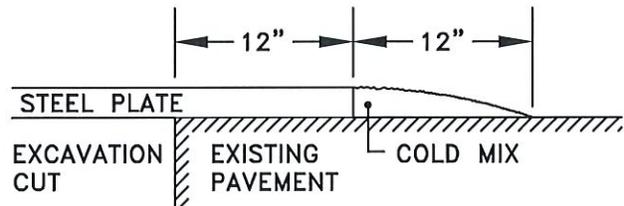
ALL STEEL PLATES MUST HAVE THE FOLLOWING INFORMATION CLEARLY AND LEGIBLY 'ETCHED' INTO THEIR TOP SURFACE:

1. OWNER'S NAME.
2. A 24 HOUR EMERGENCY CONTACT PHONE NUMBER.



NO STEEL PINS ARE PERMITTED.
SEE NEXT SHEET FOR SIGNING REQUIREMENTS

| MINIMUM THICKNESS OF STEEL PLATES | |
|-----------------------------------|-----------|
| SIZE OF PLATE | THICKNESS |
| 4' x 4' | 1/2" |
| 4' x 6' | 3/4" |
| LARGER | 1" |



CONTACT CITY OF LANCASTER DEPARTMENT OF TRANSPORTATION AT (740) 687-6668, TO REPORT THE LOCATION OF STEEL PLATES.

APPROVED 1/27/23
[Signature]
CITY ENGINEER

PAVEMENT REPAIR
STEEL PLATES

CITY OF LANCASTER, OHIO
DEPARTMENT OF ENGINEERING

STANDARD
CONSTRUCTION DRAWING

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SIGNS ARE TO BE 36"x36" FOR RESIDENTIAL AND DOWNTOWN AREAS AND 48"x48" ON MULTI-LANE, HIGH SPEED (45 MPH OR GREATER) ROADWAYS.

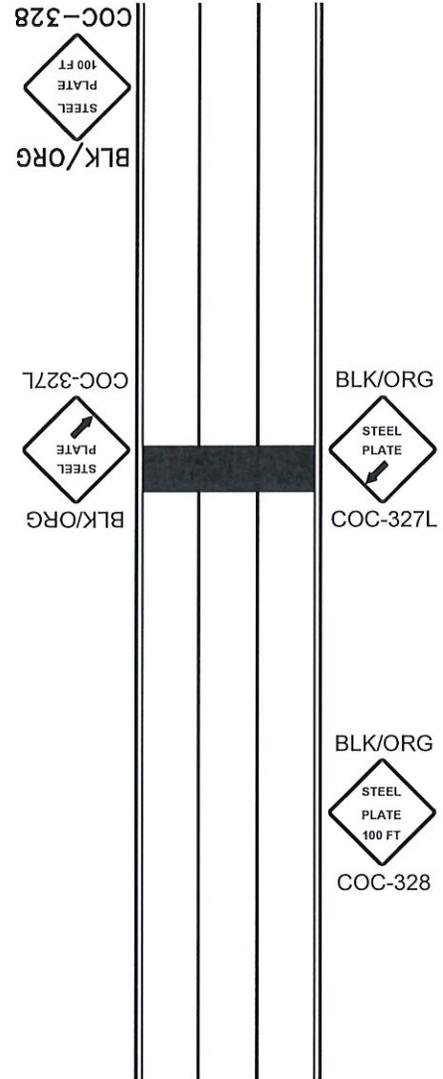
SIGN COC-327 (R/L) IS REQUIRED AT ALL PLATE LOCATIONS. SIGN COC-328 IS REQUIRED WHEN POSTED SPEED IS 35 MPH OR GREATER.

SIGNS SHOULD BE PLACED IN ALL DIRECTIONS THAT ARE AFFECTED. SIGN SPACING SHALL INCREASE TO 250' WHEN SPEED EXCEEDS 45 MPH.

SIGNS SHOULD BE DUAL MOUNTED ON MULTI-LANE, ONE-WAY ROADWAYS.

ALL SIGNS SHALL BE MOUNTED IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD).

SIGNS SHALL NOT BE PLACED IN A MANNER THAT WOULD BLOCK PARKING, BIKE LANES, OR RESTRICT A PEDESTRIAN FROM USING ANY SIDEWALK INCLUDING CURB RAMPS. PAR SHALL BE MAINTAINED AT ALL TIMES.



APPROVED 1/27/23

 CITY ENGINEER

REVISED: 27 JAN 23

PAVEMENT REPAIR
 STEEL PLATES

CITY OF LANCASTER, OHIO
 DEPARTMENT OF ENGINEERING
 STANDARD
 CONSTRUCTION DRAWING
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